

# Redcar & Cleveland Local Development Framework



## GREATER ESTON DESIGN CODE SPD

January 2010





## INFORMATION

The Greater Eston area consists of the six wards of South Bank, Grangetown, Teesville, Eston, Normanby and Ormesby (part), which has a population of almost 40,000 and consists of around 14,000 households. The Greater Eston Project is part of a long-term regeneration programme which aims to improve the housing offer, shopping facilities, educational attainment, public realm and the health and well being of its residents.



### **What?**

The Greater Eston Design Code is a set of design rules and requirements, which instruct and advise on how to plan buildings, streets and spaces. It sets out in a structured way the essential elements that must be delivered to achieve a vision. It has been adopted by the Council as part of the Local Development Framework (LDF) as a Supplementary Planning Document (SPD).



### **Why?**

The Design Code sets out the design principles to guide new development in Greater Eston. It provides general and specific parameters for the area, which must be considered as minimum requirements.



### **Who?**

The Design Code will be used by developers, their design team and the Council. The Code will be used to ensure quality and consistency across all developments in Greater Eston.

The Design Code will also be used to enhance the understanding of the wider community in terms of the vision for the area.



### **When?**

The Design Code will be a key consideration when determining any planning applications. This may mean that the Code is attached to an outline planning application, a reserved matters application or a full (detailed) application. The mechanisms to be used to formalise the Design Code are:

- Planning conditions;
- Approval as part of a planning application; and
- Planning obligations where appropriate.

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# 1.0 THE BACKGROUND

## 1.1 INTRODUCTION

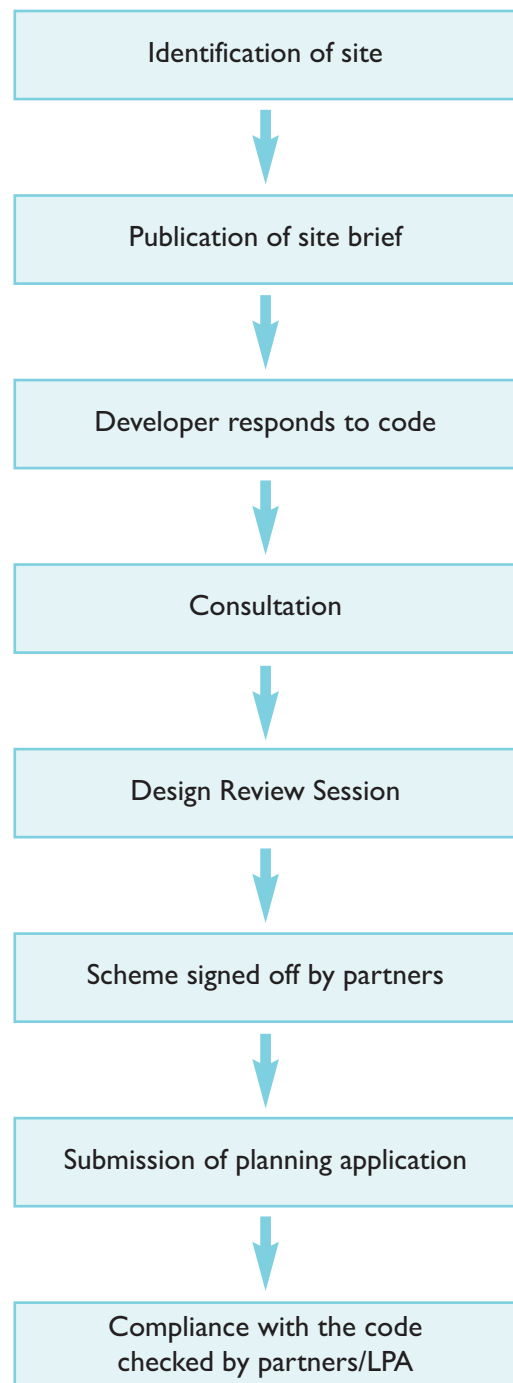
### PURPOSE

The purpose of the Design Code is to provide guidance to developers and the Council on the design of all future development in Greater Eston and to build upon the design policies already adopted by the Council in its Local Development Framework (LDF). All major development in Greater Eston will be evaluated against the principles of the Design Code.

The Design Code will be used by the Council and its partners to ensure that all new development is well designed in accordance with an agreed set of Urban Design and Building Design principles and the performance criteria as set out in the Code.

The Code will be applied throughout the design process from pre-application discussions to implementation. The process of using the Design Code in the planning process is outlined in the diagram opposite.

This Design Code is intended to be updated throughout the course of the regeneration programme and where new sites are identified.





## 1.2 BACKGROUND

The Greater Eston Project is an ambitious regeneration programme that began life in 2001 following the preparation of the Greater Eston Planning and Regeneration Strategy, which set out a vision for a regeneration programme to create a long-term sustainable future for Greater Eston.

The South Bank Housing Renewal Plan (2004) recommended the clearance of the mainly older terraced housing area to the west of South Bank and the creation of a new community based a Low Grange. This community would contain key elements needed to support a sustainable community including new homes, a new district centre and improved leisure facilities.

The Greater Eston Sustainable Communities Plan (2006) recommended a strategy for short, medium and long term interventions across all housing tenures to arrest current decline, to reduce the risk of further market failures, and to create mixed and diverse communities with a sustainable future. The report recognised the need for a Strategic Masterplan for Greater Eston.

Greater Eston is also one of the Communities and Local Government Department's Mixed Communities pilot schemes. The Mixed Communities approach aims to radically transform deprived areas into mixed and sustainable communities. This involves aligning a mix of housing, regeneration and neighbourhood renewal interventions, the collective effort of public sector agencies, targeted and sustained investment in service delivery and capital infrastructure, long term arrangements with private sector partners and a tight focus on the areas of greatest disadvantage whilst also being linked into wider physical and economic regeneration strategies.

A Strategic Masterplan for Greater Eston was prepared in 2008 to set out the long term vision for the regeneration of Greater Eston over a 15 year period. As well as the Low Grange development the Masterplan sets out priorities for regeneration including:

- selective demolition,
- refurbishment,
- new housing developments particularly in South Bank and Grangetown where sites have, or are, in the process of being cleared of failing housing, and
- public realm improvements.

The Low Grange development itself is a strategic priority in the Redcar and Cleveland Local Development Framework Core Strategy. The Low Grange Farm Supplementary Planning Document (January 2008) sets out design guidance for the proposed development.

Major investment has already taken place in Greater Eston particularly in education, with the creation of South Bank Community Primary School, Gillbrook College and a new Collaborative Sixth form on the Gillbrook Learning Campus site. The Greater Eston Health and Social Care Village will be finished in April 2010 and will house GP Services and a new library for South Bank.

## **SUSTAINABILITY APPRAISAL**

The Council is required by the Planning and Compulsory Purchase Act, 2004 to undertake a Sustainability Appraisal (SA) of the Supplementary Planning Document (SPD).

The SA includes a Strategic Environment Assessment (SEA), as required by Directive (2001/42/EC). It has been determined through the initial Habitats Regulation Assessment screening process that there is no adverse effect on the integrity of the European nature conservation sites resulting from the proposed development of Greater Eston which is the subject of this SPD.

The SA has been undertaken during the preparation of the SPD and has helped to inform its development. The appraisal is published alongside the SPD and is available on the Council's website at:

<http://www.redcar-cleveland.gov.uk/LDF>

## I.3 PLANNING FRAMEWORK

### Regional Spatial Strategy

The North East of England Regional Spatial Strategy 2008 (RSS) promotes a pro-active approach to delivering economic growth and the renaissance of the region. Tackling the problems of poor housing and deprivation in order to support the creation of sustainable communities is a priority theme. The following policies are relevant to the Design Code:

<b>Policy 1</b>	North East Renaissance
<b>Policy 2</b>	Sustainable Development
<b>Policy 10</b>	Tees Valley City Region
<b>Policy 25</b>	Urban and Rural Centres
<b>Policy 28</b>	Gross and Net Dwelling provision
<b>Policy 30</b>	Improving Inclusivity and Affordability

### Redcar and Cleveland Local Development Framework

Redcar and Cleveland Local Development Framework (LDF) Core Strategy and Development Policies DPDs were adopted by the Council in July 2007. The relevant policies to this Design Code are:

### Core Strategy

<b>Policy CS1</b>	Securing a Better Quality of Life
<b>Policy CS3</b>	Spatial Strategy for Greater Eston
<b>Policy CS15</b>	Delivering Mixed and Balanced Communities and Quality Homes
<b>Policy CS17</b>	Housing Density
<b>Policy CS18</b>	Town, District and Local Centres
<b>Policy CS19</b>	Delivering Inclusive Communities
<b>Policy CS20</b>	Promoting Good Design

### Development Policies

<b>Policy DP3</b>	Sustainable Design
<b>Policy DP4</b>	Developer Contributions
<b>Policy DP5</b>	Art and Development

The Low Grange Supplementary Planning Document was adopted in January 2008 and sets out Redcar and Cleveland Borough Council's requirements for the development of the site at Low Grange Farm in South Bank.

## I.4 THE VISION

The Greater Eston Masterplan creates a vision for the regeneration of Greater Eston to build a desirable, high quality and attractive place to live, which will improve the opportunities for existing residents and attract new people and investment.



The vision for Greater Eston is:

*Together we will work to capitalise upon Greater Eston's strategic location close to the River Tees employment corridor to maximise opportunities for business growth.*

*The aim is to create high quality, well designed, attractive and sustainable new communities with areas of new and improved housing, a wider choice of properties and tenures, an attractive environment and improved community facilities and services.*

## 1.5 BUILDING A BETTER BOROUGH

The Redcar and Cleveland Local Strategic Partnership (LSP) has agreed priorities for tackling the challenges facing the Borough. These are set out in Redcar and Cleveland's Sustainable Community Strategy 2008 - 2021 "Building a Better Borough" summarised in the table below:

Priority Theme	Objectives
Sustainable Communities	<ul style="list-style-type: none"> <li>To develop the Borough's economy and regenerate communities</li> <li>To achieve a high quality and sustainable living environment</li> <li>To promote culture within the Borough and improve access to leisure and cultural services</li> </ul>
Safer Stronger Communities	<ul style="list-style-type: none"> <li>To achieve a safer community</li> <li>To achieve a stronger community</li> </ul>
Healthier Communities and Older People	<ul style="list-style-type: none"> <li>To improve the health and well being of the population and reduce health inequalities</li> <li>To establish an integrated holistic approach to planning, commissioning and delivery of services for children and young people</li> </ul>
Children and Young People	<ul style="list-style-type: none"> <li>To help children, young people and their families to be healthy</li> <li>To help children and young people to stay safe</li> <li>To help children and young people to enjoy and achieve</li> <li>To help children and young people to make a positive contribution</li> <li>To help children, young people and their families achieve economic well being</li> <li>To establish an integrated, holistic approach to planning, commissioning and delivery of services for children and young people</li> </ul>
Corporate Efficiency, Quality and Equality	<ul style="list-style-type: none"> <li>To improve the quality and efficiency of services and secure value for money from Council resources</li> <li>To be an employer of choice</li> <li>To communicate effectively with all stakeholders and ensure transparency and democratic accountability</li> <li>To promote equality and recognise diversity</li> </ul>

## I.6 OUR PLAN

'Our Plan' is the Council's key corporate planning document which outlines the things the Council is going to do to make the Borough an excellent place to live, learn, visit, work and invest in. The plan is based around 6 priority themes and contains 14 key objectives. These are summarised in the table below:

Priority Theme	Objectives
Children and Young People	<ul style="list-style-type: none"> <li>• Improve educational achievement among young people</li> <li>• Increase the number of young people in education, employment, training or through improved engagement</li> <li>• Safeguard our children and young people</li> </ul>
Healthier Communities and Older People	<ul style="list-style-type: none"> <li>• Reduce health inequality</li> <li>• Transform services for adults and older people</li> </ul>
Safer Communities	<ul style="list-style-type: none"> <li>• Tackle anti-social behaviour and reduce criminal damage</li> </ul>
Stronger Communities	<ul style="list-style-type: none"> <li>• Re-design our services to improve neighbourhood delivery in disadvantaged communities</li> </ul>
Sustainable Communities	<ul style="list-style-type: none"> <li>• Tackle worklessness and poverty</li> <li>• Deliver improvements in our towns and villages</li> <li>• Create a regeneration blue print for the whole Borough</li> <li>• Reduce our carbon footprint</li> <li>• Grow our tourism potential to deliver economic and cultural opportunities</li> <li>• Promote an integrated, efficient public transport system</li> </ul>
Business Improvement	<ul style="list-style-type: none"> <li>• Deliver our top 10 business improvement projects</li> </ul>

## I.7 A NEW PERSPECTIVE

The document 'A New Perspective' sets out the delivery plan in Greater Eston with 5 key areas for intervention and action:

- Theme 1: Create homes for the future;
- Theme 2: Develop spaces to breath and places to be proud of;
- Theme 3: Create healthy and safe communities with plenty to do;
- Theme 4: Ensure Greater Eston is prosperous and skilled; and
- Theme 5: Shouting about it.

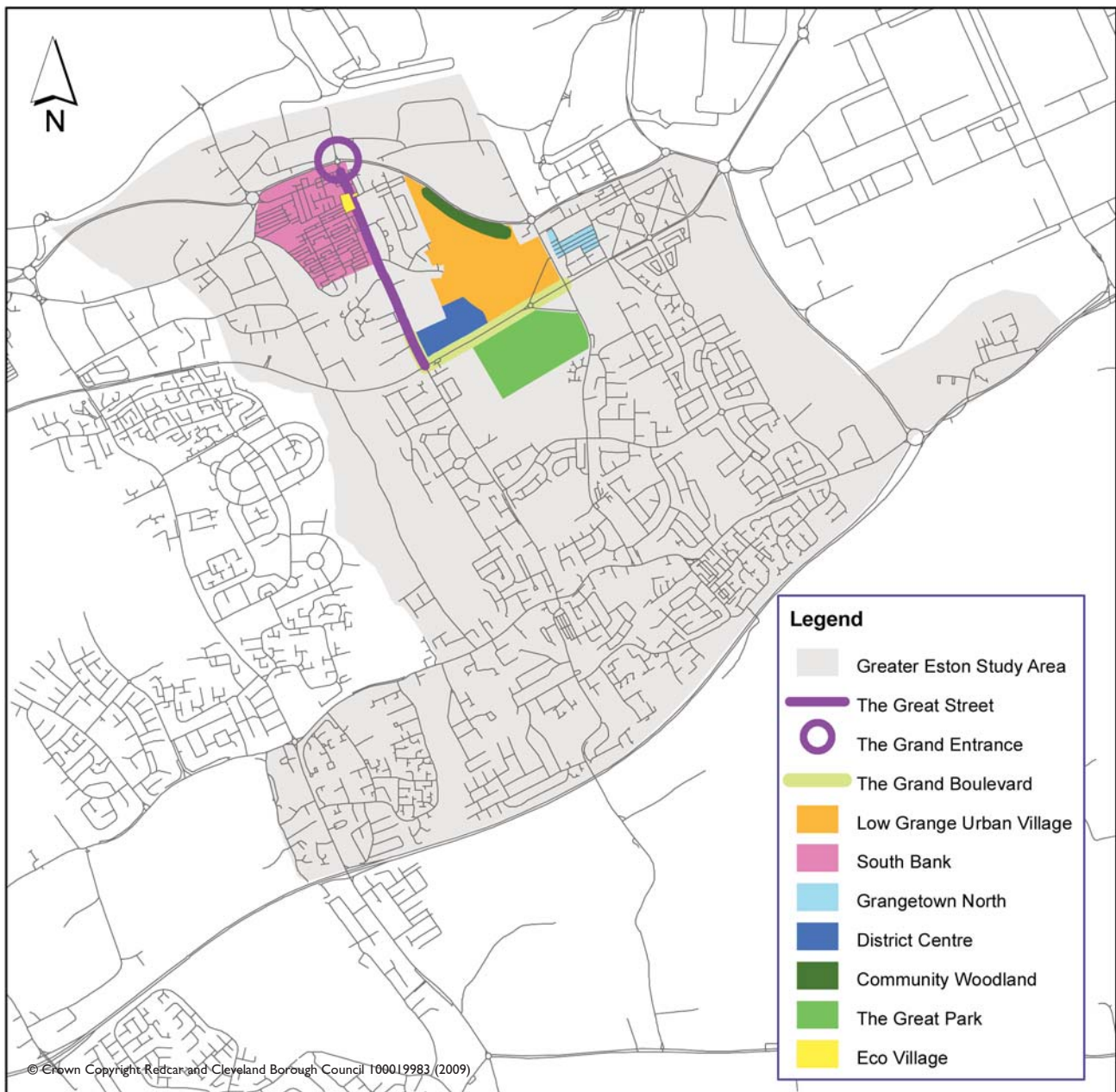
The plan outlines the vision, opportunities and proposed interventions to improve Greater Eston and forms the basis for attracting and monitoring investment. Its purpose is to provide information on future strengths and weaknesses, to look at how those weaknesses will be tackled and what opportunities will be seized on to maximise the area's strengths.

## 2.0 GREATER ESTON PROJECTS

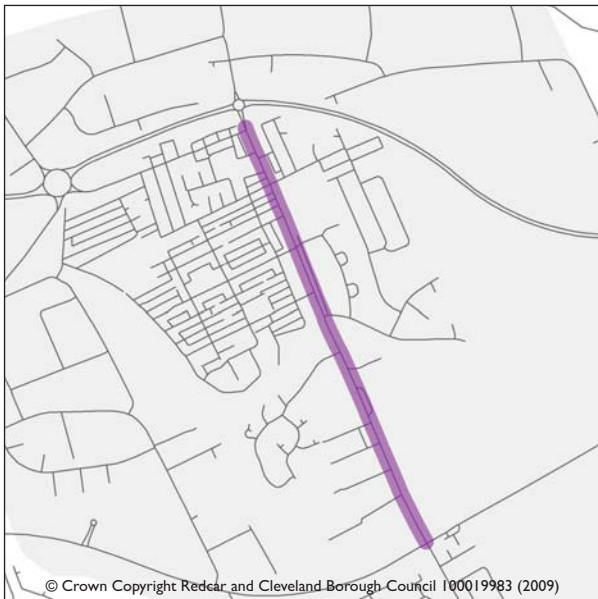
A number of key projects are proposed to assist in the regeneration of the Greater Eston Area. These projects are outlined on the map below within the context of Greater Eston.

The following pages provide a brief overview of current proposals and set out design guidance to show what is expected to be delivered on each site. These sites will be reviewed and updated when new projects within Greater Eston are identified.

### Greater Eston Projects Plan



## 2.1 THE GREAT STREET - Normanby Road



Normanby Road is an important road through South Bank. The visual condition and the streetscape of the northern part of the road are poor. Public realm improvements are needed that incorporate parking, along with measures to upgrade the quality of shop frontages.

### ***Development proposals for Normanby Road should comply with the following guidance:***

*A consistent arrangement of regularly planted street trees should follow Normanby Road wherever possible, to provide a buffer between traffic and pedestrians.*

*Pavement and public realm improvements must be carried out to attract more pedestrian movement and enhance the quality of Normanby Road.*

*Shop fronts and residential frontages, where possible, should undergo face-lifts to a consistent standard using an agreed palette of materials and detailing.*

*The existing good quality traditional buildings within the streetscene should be retained and enhanced, where possible, to strengthen the sense of place.*

*Short-stay car parking and improved vehicle and pedestrian access is essential to enable local businesses to capture passing trade.*

**A New Perspective - Theme 2:**  
*Develop spaces to breathe and places to be proud of*

## 2.2 THE GRAND ENTRANCE - The Northern Gateway



This area is the main entrance to South Bank and Teesville from the A66. The poor quality of the urban fabric gives a poor perception of the area to residents and businesses.

The area is characterised by a variety of buildings types and styles, which form the frontage onto Normanby Road and the A66 junction. The urban blocks vary in scale and size with a number of gaps in the streetscape. Building frontages are generally in a poor state of repair.

**Development proposals for the Northern Gateway should comply with the following guidance:**

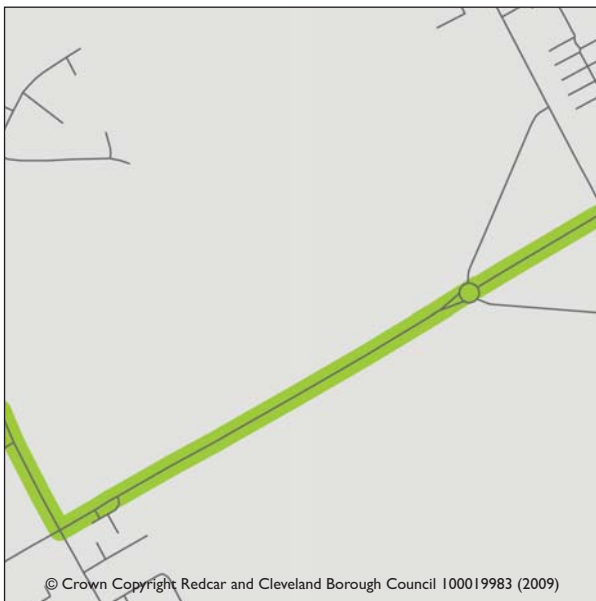
*A new gateway should be formed that creates a 'sense of arrival' strengthened by the strategic arrangement of new landmark buildings at the entrance of the Normanby Road and A66 junction.*

*Public realm and property frontages should be improved, together with infill development to create a more coherent and attractive entrance to Greater Eston.*

*Highways and landscape improvements should aim to enhance the quality of the entrance to Normanby Road.*

**A New Perspective - Theme 2:**  
*Develop spaces to breathe and places to be proud of*

## 2.3 THE GRAND BOULEVARD - Trunk Road



The A1085, known locally as the Trunk Road, is a significant route into and through Greater Eston which carries a wide variety of traffic serving the residential and commercial areas of Greater Eston. At present the Trunk Road is utilitarian in character and is perceived as a major highway route through Greater Eston.

The Design Code provides for the redesigning of the Trunk Road as an urban boulevard to better accommodate patterns of movement and become a street that people can enjoy being in, connected through a network of spaces and routes.

### **Development proposals for the Grand Boulevard should comply with the following guidance:**

*The Grand Boulevard should provide an enjoyable and attractive street designed as a place in its own right. A consistent arrangement of regularly planted street trees should follow the Trunk Road wherever possible to form a boulevard separating traffic from pedestrians.*

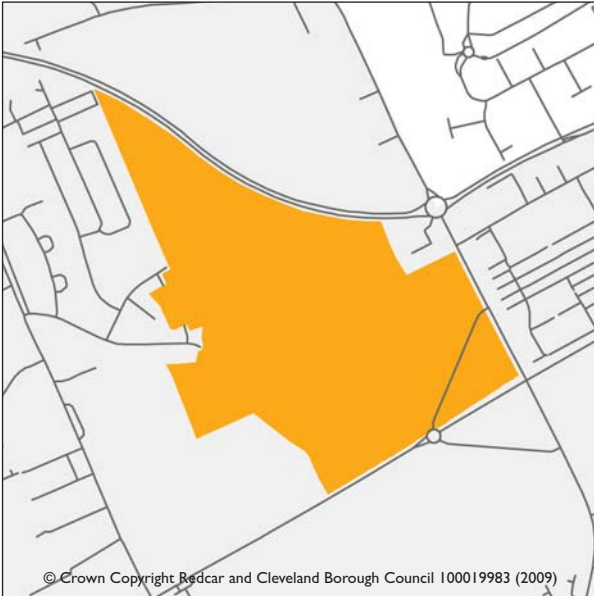
*Buildings should face the boulevard and be designed to create a strong and distinct character to the boulevard, whilst another contrasting character may be established behind the boulevard frontage. A consistent set of materials and detailing should be used to reinforce the character of each area.*

*The Grand Boulevard will be a multi-purpose route, with specific provision for varied transport modes including cycles, pedestrians and public transport and motor traffic. No in-curtilage parking accessed directly from the boulevard will be permitted.*

*Public art will be a key component of the Grand Boulevard, and may take a variety of forms including lighting, surface treatments, signage, colour. Specific guidelines on street specifications and street typology are available in Chapter 4 - Spatial Structure.*

**A New Perspective - Theme 2:**  
*Develop spaces to breathe and places to be proud of*

## 2.4 LOW GRANGE URBAN VILLAGE



Low Grange covers an area of approximately 46 hectares and is mainly vacant grassland, part of which was previously developed for residential purposes, but has now been cleared and returned to grassland. The land uses within the site include part of the St Peter's Catholic College of Maths and Computing, playing fields, a local library, post office sorting depot, a telephone exchange and land on which a former farmhouse and petrol filling station were sited.

The site is relatively flat and is mainly open in character. There are trees and hedgerows along the watercourses and scattered along the boundaries of the site.

The Low Grange Farm Supplementary Planning Document (SPD) 2008 has been adopted by the Council. This sets out general advice on design and layout.

Within the Low Grange residential neighbourhood a number of key areas have been

identified to support the vision and planning framework identified in the Low Grange Farm SPD.

The Urban Village forms the largest part of Low Grange. The site covers an area of approximately 27 hectares with a potential capacity of over 1000 residential units.

### **Development proposals for the Low Grange Urban Village should comply with the following guidance:**

*The urban village should be designed to create a neighbourhood that is easy and safe to move around in, particularly for those with mobility difficulties, providing good access to the new district centre, health village, schools, and adjacent Eston Park.*

*Buildings should be formed into clearly defined blocks with secure gardens or courtyards to the rear.*

*Buildings should generally be 2 and 2 ½ storeys high, with opportunities for 3 or higher at key locations such as the corner of blocks, end of vistas or where the street scene permits.*

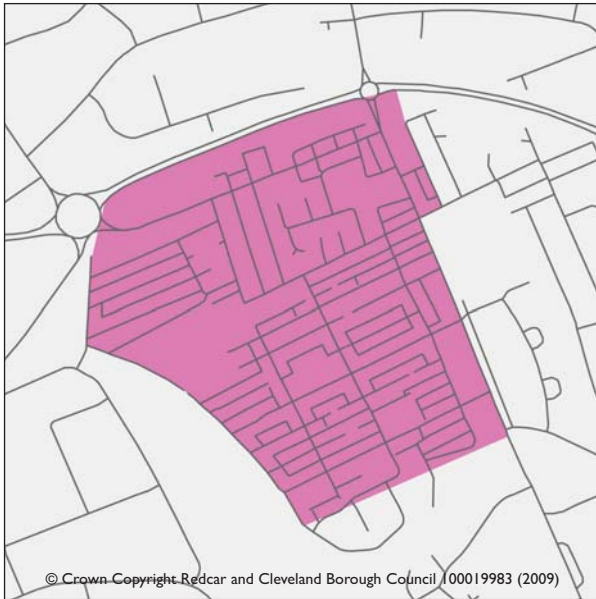
*The design of streets in the urban village shall be based on the principles of Manual for Streets. Specific guidelines on street design and typology are available in Chapter 4 - Spatial Structure.*

*New housing should incorporate a variety of design characteristics derived from local and regional vernacular. Buildings should utilise a simple palette of materials and architectural detailing. The result will be a development that is aesthetically pleasing based on simple historic design rules i.e. symmetry and proportion. Specific information on materials and detailing is available in Chapter 5 – Detailing the Place.*

**Refer to Design Code Checklist - Appendix I.**

**A New Perspective - Theme 1:  
Create homes for the future**

## 2.5 SOUTH BANK



South Bank covers an area to the west of Normanby Road, which is characterised by mainly older terraced pre 1920s housing. This area has suffered housing market failure. In 2006, the Council adopted the South Bank Housing Renewal Plan, which dealt with the worst of the residential blocks in terms of void levels and condition. Over 350 properties have now been demolished. A commitment was also given to monitor the area of older terraced housing that remained in order to understand what the problems were and establish if further demolition was needed.

Potential sites of approximately 10 hectares may become available in the South Bank area for redevelopment as part of this housing clearance. New housing will be required to provide high quality accommodation for both current and new residents.

### **Development proposals for South Bank should comply with the following guidance as the area is redeveloped:**

*Maximise the opportunity to renew the area's identity through a coherent set of design ideas which refines the existing built form and layout of South Bank.*

*Wherever practical, improved connectivity between Normanby Road and existing residential streets should be provided.*

*Any residential development must demonstrate Homezone principles. See Chapter 4 - Spatial Structure.*

*Renewal of existing roads, paving and boundary treatments is encouraged in order to improve the quality of existing streets.*

*Buildings facing onto Normanby Road should be improved, wherever possible, in order to enhance the quality of the frontage onto this route.*

*Wherever redevelopment is proposed it should focus on creating perimeter block layouts that form well connected street spaces with strong 'active' frontage elevations and secure private space.*

*Proposals for new development should follow the guidelines set out for the Low Grange Urban Village and Design Code Checklist - Appendix 1.*

**A New Perspective - Theme 1:  
Create homes for the future**

## 2.6 GRANGETOWN NORTH



This is a large brownfield site of approximately 3.24 hectares, close to the proposed mixed-use regeneration site at Low Grange, located to the south of the A66 and to the east of Church Lane in Grangetown. It was cleared of over 200 terraced houses as part of the housing renewal programme for the area.

The redevelopment of the site is critical to support the regeneration of this part of Grangetown.

### **Development proposals for Grangetown should comply with the following guidance:**

*A renewed identity for the area should be established by enhancing access to Grangetown and forming new high quality public spaces.*

*Wherever redevelopment is proposed it should focus on creating perimeter block layouts that form well connected street spaces with strong 'active' frontage elevations and secure private space.*

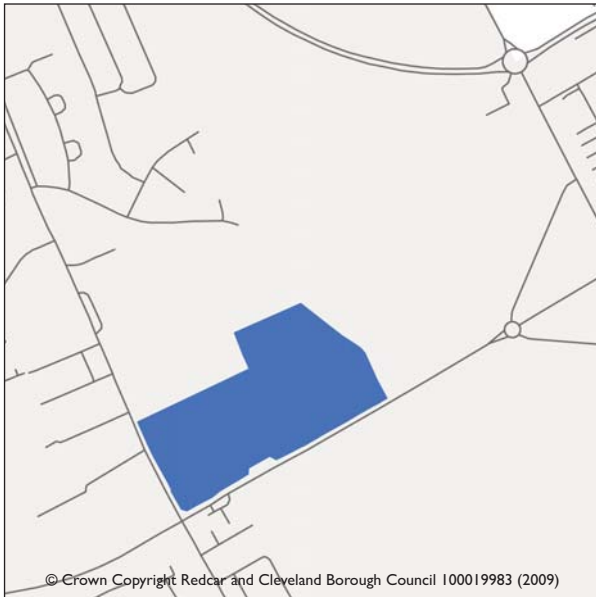
*Any residential development must demonstrate Homezone principles. See Chapter 4 - Spatial Structure.*

*A landscape buffer should be maintained between Eston Road and North Grangetown renewal area.*

*Proposals for new development should follow the guidelines set out for the Low Grange Urban Village and Design Code Checklist - Appendix 1.*

*A New Perspective - Theme 1:  
Create homes for the future*

## 2.7 LOW GRANGE DISTRICT CENTRE



The District Centre will cover an area of approximately 11.2 hectares. The site lies in a prominent position adjacent to a major road junction of Normanby Road and the A1085 (Trunk Road).

The District Centre is intended to create a new heart for the area, containing retail, healthcare and community facilities and a mix of uses at the centre of the community with good links to schools and high quality open space.

The Low Grange SPD sets out guidance on the layout of the district centre. A new retail store will act as an anchor for the new district centre, as well as a range of smaller shops to support the local community. A health village will also be provided fronting onto Normanby Road. The health village will provide general practice facilities as well as a range of specialist health care and primary care uses and a care home. In addition, a new library and one-stop-shop will be provided by the local authority as part of the proposals.

### **Development proposals for the District Centre should comply with the following guidance:**

*The District Centre must form a 'gateway' to Low Grange. Its location at the junction of Normanby Road and the Trunk Road should be exploited to maximise its potential. Any development will be expected to be of an excellent quality and make the best use of its location by creating a strong and attractive frontage onto Normanby Road and the Trunk Road.*

*The narrow and constrained nature of the gateway site on the south western corner of the District Centre will require a suitable bespoke solution to address both operational issues and access from the District Centre and Trunk Road. A building of excellent quality which creates a focal point and faces the street and public realm will form part of this response.*

*The design of the District Centre will be imaginative and demonstrate innovation through the use of sustainable building technologies and best practice.*

*Parking and highways infrastructure should be kept to a minimum. Car parking must be well integrated into the scheme.*

*Safe, strategic pedestrian and cycle routes between the district centre and the surrounding residential areas should form key elements in the site layout.*

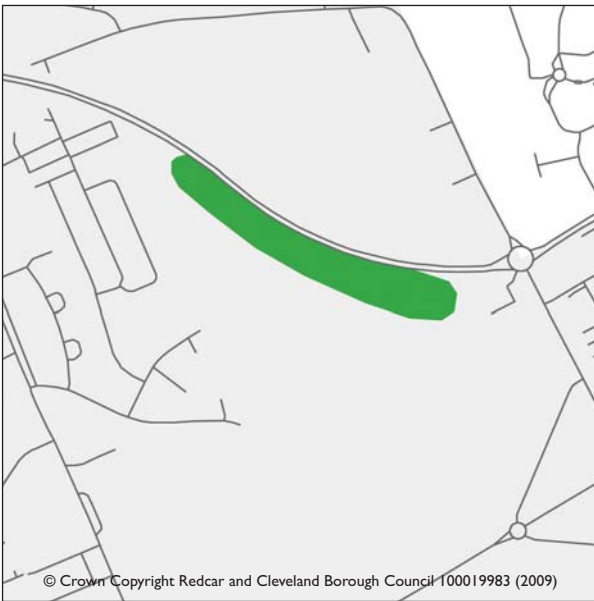
*A new public square must be provided as a key element of the scheme to provide access to all facilities and potential for events and activities.*

*'The district centre will be expected to incorporate artistic features and landscaping which are consistent with the principles set out in the Greater Eston Public Arts Strategy.'*

### **Refer to Design Code Checklist - Appendix 1.**

**A New Perspective - Theme 3:**  
*Create healthy and safe communities with plenty to do*

## 2.8 LOW GRANGE COMMUNITY WOODLAND



The Community Woodland covers an area of approximately 5.6 hectares adjacent to the northern edge of Low Grange Farm. The woodland will act as a landscape buffer between the new residential neighbourhood and industrial development to the north.

The space should provide a new community woodland, amenity space, opportunities for local biodiversity enhancement and help to screen views of industrial areas to the north of Low Grange.

### **Development proposals for the Community Woodland should comply with the following guidance:**

*The Community Woodland will range from a minimum of 20 to 60 metres in width measured from the southern edge of the footpath/cycleway running parallel to the A66.*

*The existing planted bund on the south side of the A66 must be retained as a noise baffle and visual screen. There are some areas of recent tree planting to the south side of the bund that should also be retained where they occur within the proposed community woodland or where anticipated land levels allow for their retention.*

*Within and on the edge of the community woodland, a series of amenity spaces should be created, connected to the existing footpath / cycleway. Consideration should be given to some additional path routes to better serve the amenity areas while maintaining a strong link between South Bank and Grangetown.*

*Woodland must include native trees, together with some native understorey shrub planting and the creation of a sinuous or scalloped woodland edge to create a variety of habitats. Planting must consist of a range of plant sizes, from whips to feathered trees to create a varied planting structure and benefit the wildlife.*

*Proposals will be expected to provide multifunctional green space, including opportunities for the creation and enhancement of habitats.*

**A New Perspective - Theme 2:**  
*Develop spaces to breathe and places to be proud of*

## 2.9 THE GREAT PARK



A new urban park will be created at Eston Recreation Ground covering an area of approximately 24 hectares. The Great Park will provide a central hub for recreation and leisure activities for the whole of the Greater Eston community and visitors.

The new urban park will also provide the opportunity to protect and enhance the biodiversity of the area, linking into the existing green infrastructure network.

### **Development proposals for the Great Park should comply with the following guidance:**

*The park must be landscaped to a high quality both providing and linking into the existing green infrastructure network.*

*Entrance points at the Trunk Road, Normanby Road and Church Lane should be created and retained.*

*The layout should provide for both vehicular access where required and safe pedestrian and cycle paths incorporated into existing routes.*

*Subject to consultation, proposals for the Great Park are anticipated to include: new facility buildings, new landscaping and mounding, events space and playgrounds.*

*Proposals will be expected to provide multifunctional green space, including opportunities for the creation and enhancement of habitats.*

*The design of all structures and landscaping will need to be linked to the principles set out in the Greater Eston Public Arts Strategy.*

**A New Perspective - Theme 2:**  
*Develop spaces to breathe and places to be proud of*

## 2.10 THE GREEN HEART ECO VILLAGE



The Green Heart, Eco Village site will cover approximately 0.6 hectares within South Bank. The site comprises of two separate parts; with the main section of the site being located to the west of Normanby Road, bounded by King George's Square to the north and the smaller section a terrace of five properties to the east of Normanby Road on Redcar Road East, overlooking St John's Church.

The proposal is to create an Eco Village which will help to stimulate further investment in South Bank and act as a catalyst to the regeneration of the housing market in South Bank as a whole.

The scheme is expected to comprise 12 new buildings, to the north of the site and the refurbishment and remodelling of two existing terraces to the south of the site and the terrace of 6 dwellings on Redcar Road East.

A Development Brief for the site has been prepared.

### **Development proposals for the Eco Village should comply with the following guidance:**

*To the north of the site a mixture of new houses should be provided, built to a very high design standard and with a clear identity with an emphasis on 3 and 4 bedroom family homes. The building designs should be unique, innovative and accord with sustainability principles. The dwellings are expected to be carbon-neutral, achieving level 6 on the Code for Sustainable Homes, with the whole scheme aiming to achieve at least a Silver Standard under Building for Life.*

*Extensive renovation works to the existing dwellings at the south and east of the site, will be required, including both internal and external works. The properties to the south of the site should be retained and refurbished to high design quality and eco standards.*

*Approximately 30% of these dwellings should be affordable and it should be designed so that all residents have access to digital technology.*

*The 6 properties to the east on Redcar Road East should be refurbished to a high standard to provide office/workspace/retail space. The proposals should provide an innovative solution with high eco credentials.*

*The scheme should provide a robust and high quality landscape environment both within the site and bordering the sites, including the streets.*

**A New Perspective - Theme 1:**  
*Create homes for the future*



# 3.0 CREATING THE URBAN STRUCTURE

## 3.1 DENSITY

The density of new development should relate to local context and the need to provide a mix of house types and densities.

In order to create a socially mixed community with varied lifestyles, Greater Eston requires a choice of building types and settings. It is possible to achieve this by not grouping too many of the 'lower' density units together and by creating a fine-grained pattern of development.

Density is a measurement of the number of dwellings, which can be accommodated on a site or in an area. PPS3 states that the density of existing development should not dictate that of new housing by stifling change or requiring replication of existing style and form. If done well imaginative design and layout of new development can lead to more efficient use of land without compromising the quality of the local environment.

Development will be expected to achieve a density of 30-50 dwellings per hectare, with densities at the higher end of the scale expected close to the proposed District Centre at Low Grange, local centres and public transport routes.

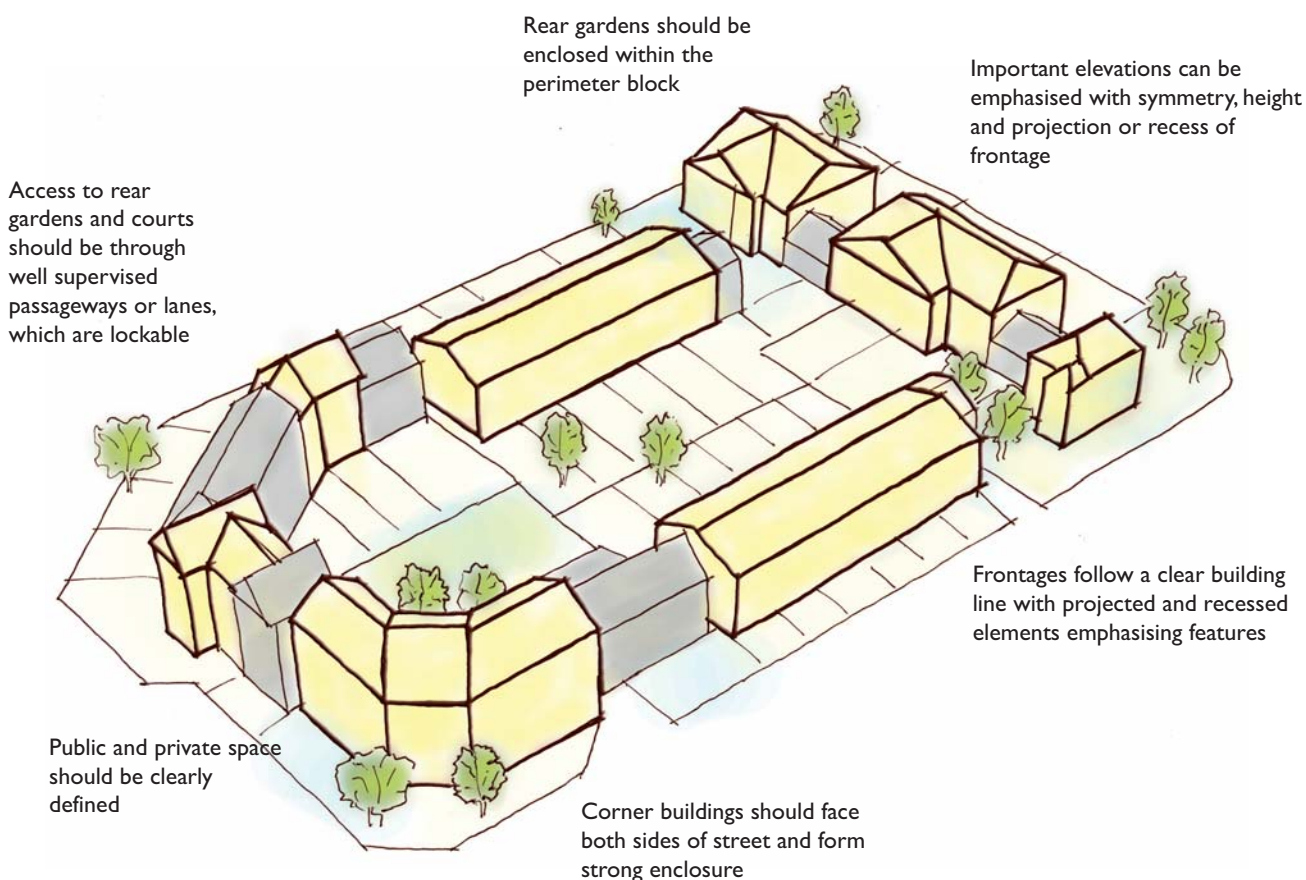


## 3.2 BLOCKS, SIZE, TYPE AND SHAPE

### Blocks

The development block is the land area defined by the grid. It can vary considerably in shape and size according to the configuration of streets, preferred orientation and topography, as well as the nature of plot sub-divisions and building types that are to be accommodated.

The principles of the block, set out in this section are the basic building blocks of good design in Greater Eston. The principles apply to both new and re-configured sites.

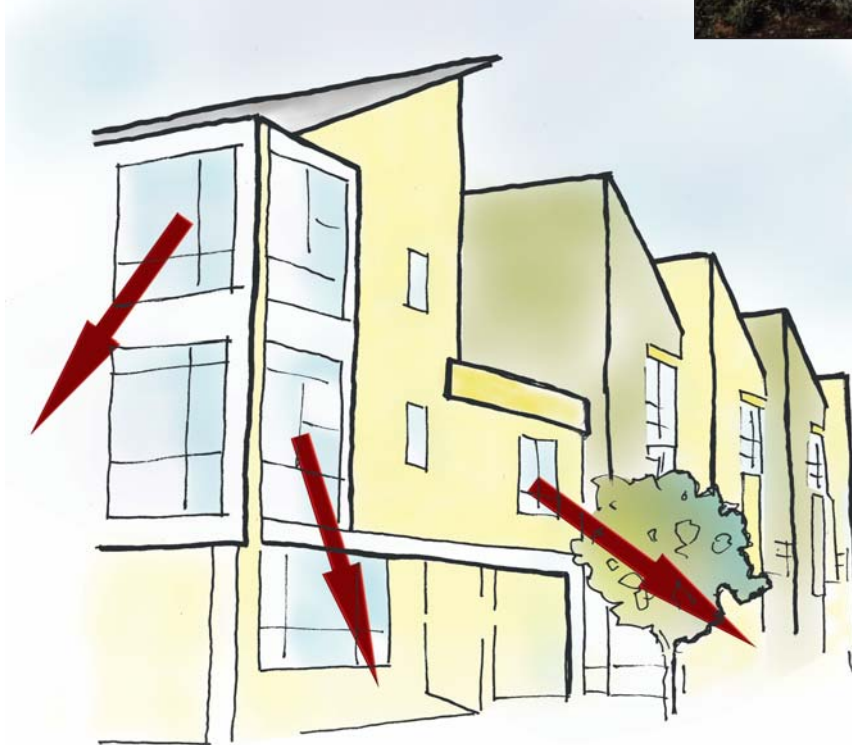


## Face the Street

The most fundamental requirement of structuring built form within development blocks is to make a clear distinction between public fronts and private backs. Buildings should front on to the street, squares and open spaces in order to give life to it. Corner units should also overlook more than one street/ space to create a safer environment.

## Continuity of Building Line

Continuous building lines along the frontage can provide good enclosure to a street or square, generating an 'active edge' with frequent doors and windows creating design features. A consistent building line should be achieved along most of the length of a frontage with the majority of properties achieving a similar setback.

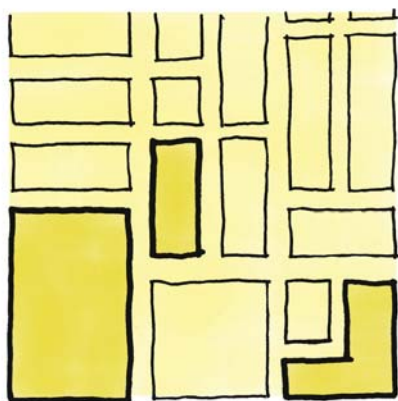


Active frontage onto street enables good natural surveillance and gives a strong sense of place.

## Block Size

In considering the size of development blocks a balance has to be reached between existing block sizes; ease of access; the ability to sustain a variety of building types and sizes; and the ability for a place to change and adapt over time. Existing blocks in Greater Eston vary in size and shape with many being relatively small and separate from other blocks. This has created a very permeable network of footpaths and routes with poor natural surveillance creating security and safety concerns.

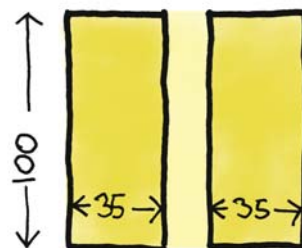
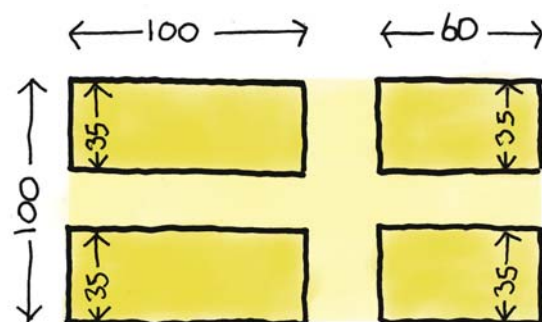
There may be an opportunity to create larger blocks or consolidate smaller blocks to overcome these issues.



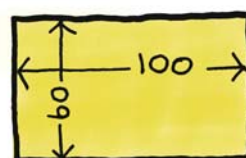
Development blocks can accommodate a range of building sizes and densities

## Block Shape

Square blocks will generally offer the most flexible basis for accommodating a range of building types and options for internal treatment. The shape of blocks should also respond to new and existing road networks while taking account of the topography and orientation of the sites.



Sub-division into narrower residential blocks



Amalgamation into larger residential block

# 4.0 SPATIAL STRUCTURE

## 4.1 STREET DESIGN

The needs of people on foot must be taken into account in the design and layout of all new development. Development proposals and area improvements in Greater Eston should use the principles of Manual for Streets for the design, construction and maintenance of new residential streets. It is important that designers place high priority on meeting the needs of pedestrians, cyclists and public transport users, so that people can move around the community easily without having to rely on the car.

It is imperative that streets are designed as complete public spaces with consideration given to the entirety of the street including roads, parking, pavements, property boundaries, public space and even the facades of the buildings fronting onto streets. Ease of access for all is a key objective and all new paths and streets should be designed in accordance with the Disability Discrimination Act.

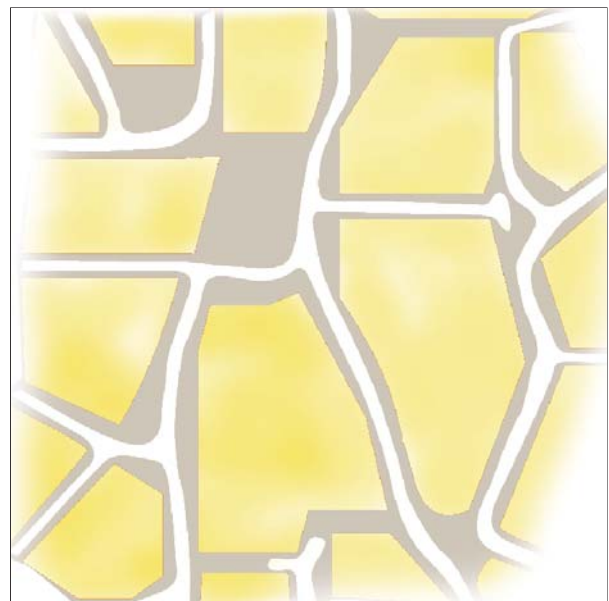


### Street Typology

The types of streets to be used in Greater Eston form the key to its overall character. Four street typologies have been identified based on the design of the streets as a place as well as the function of the road in movement terms. These are:

- 1 Boulevards**
- 2 Residential Streets**
- 3 Home Zones**
- 4 Mews and Courtyards**

The design of these streets should respond to their role in the street hierarchy and their position within the urban form, by responding to public spaces and linking spaces and destinations. The following pages set out street specifications by street types.



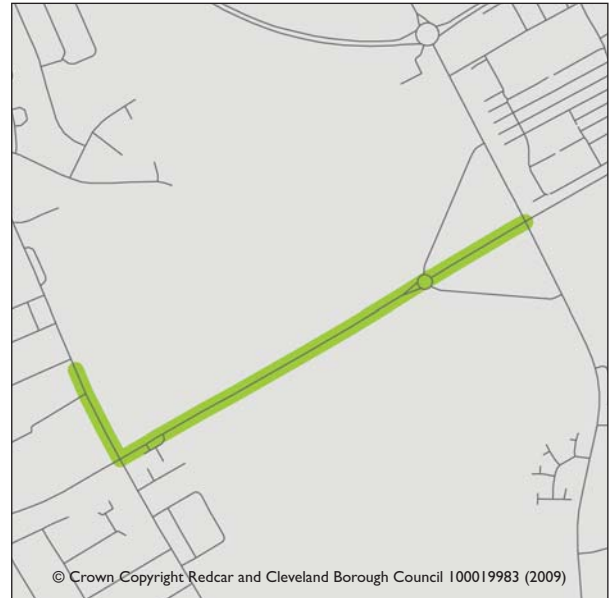
## Boulevards

The Boulevards forms the main movement network through Low Grange, providing primary routes between home, work, shopping and leisure within Greater Eston.

### Key Components

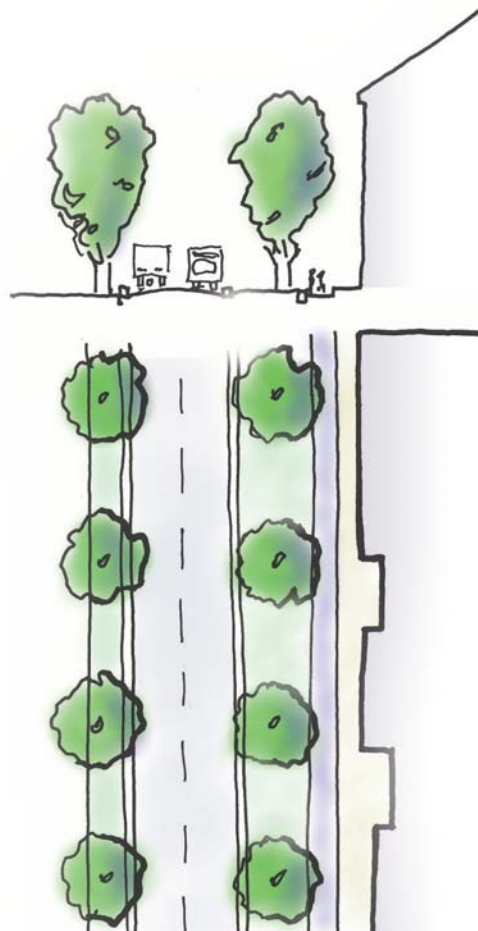
The boulevard treatment is made up of the following key components:

- Planting of specimen trees to form a double row of trees (or a single line where space is restricted).
- Building frontages facing onto Normanby Road and Trunk Road.
- Provision for vehicles, pedestrians and cyclists to all safely use the street.



## Design Parameters

Public	
Speed limit	30mph
Carriageway width	7.3m typical standard but will vary on existing roads + 1.5m cycle + 2 - 3m footway
Junction spacing	40m on opposite sides, 70m on same side
Corner radii	10m at main streets, 6m at local and side streets
Tree Planting	<p>Trees planted as extra-heavy standards (14cm in girth and 4m or more in height). Detailed specification will be required including use of linear 'root barriers' or 'root guards' to protect underground services and adjacent surfaces.</p> <p>Species may be selected from the following list; choice depends on location and space. The aim is for some (but not total) uniformity ie for substantial groups consisting of single species.</p> <p>Large trees:</p> <ul style="list-style-type: none"> <li>Acer platanoides 'Columnare'</li> <li>Carpinus betulus 'Fastigiata'</li> <li>Tilia cordata 'Green Spire'</li> <li>Tilia Platyphyllos 'Fastigiata'</li> </ul> <p>Medium trees:</p> <ul style="list-style-type: none"> <li>Pyrus calleryana 'Chanticleer'</li> <li>Sorbus x thuringiaca 'Fastigiata'</li> <li>Acer campestre 'Elegant'</li> <li>Acer campestre 'Arends'</li> <li>Sorbus aucuparia 'Joseph Rock' and similar.</li> </ul>
Private	
Building height	2 storeys generally, 3 storeys at corners and vistas
Plot width	4m minimum
Frontage on building line	80 - 100%
Front garden / setback	2 - 3m



**Boulevard**  
 Typical standards apply with tree planting, cycleway and footway.

**Suggested Materials Pallet \***

Carriageway Surface	Footway / Cycle Lane Surface	Parking bays / Crossings	Carriageway Kerbs
Bituminous (black / grey)	Resin bound gravel	Tegula Cobbles (dark blue)	Textured Concrete Kerb (grey)
			

\* Developers will be expected to use the materials in the suggested materials pallet, unless suitable alternatives are chosen which either match or exceed the quality of those suggested.

## Residential Street

Residential streets connect the Boulevards and the main routes through the residential areas of Greater Eston and Low Grange. Their character should be designed to accommodate a wide range of uses including public transport services, traffic movement and residential activities.

### Key Components

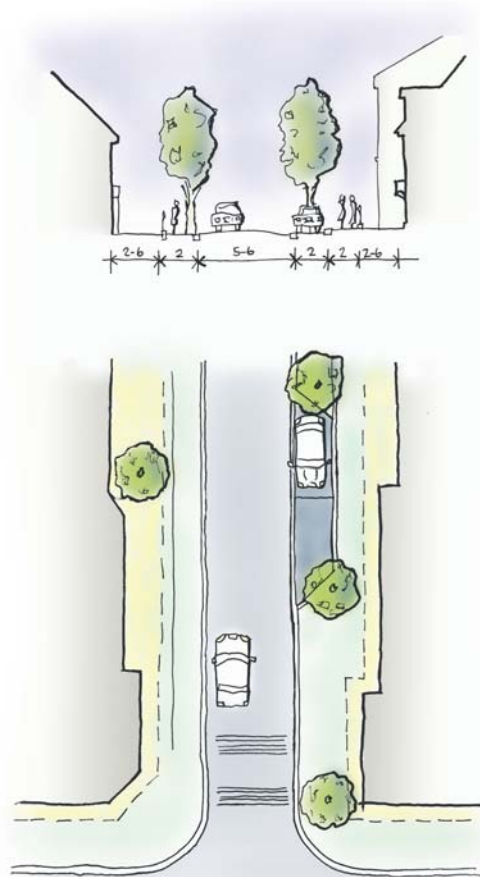
The Residential Street treatment is made up of the following key components:

- Tree planting along the road way and in build outs.
- Building frontages and entrances to face onto road ways to create pinch points where possible.
- Continued existence of strong boundary definition with wall, railings and hedgerows.
- Consistent use of high quality materials on all surface treatments.



## Design Parameters

<b>Public</b>	
Speed limit	20 - 30mph
Carriageway width	5 - 6 m typical maximum and 1 x 2m footway
Junction spacing	40m on opposite sides, 70m on same side
Corner radii	6m at residential and local streets
Tree Planting	<p>Species may be selected from the following; choice depends on location and space. Aim is for some (but not pure) uniformity ie for substantial groups consisting of single species.</p> <p>Large trees:  Acer platanoides 'Columnare'  Carpinus betulus 'Fastigiata'  Tilia cordata 'Green Spire'  Tilia Platyphyllos 'Fastigiata'</p> <p>Medium trees:  Pyrus calleryana 'Chanticleer'  Sorbus x thuringiaca 'Fastigiata'  Acer campestre 'Elegant'  Acer campestre 'Arends'  Sorbus aucuparia 'Joseph Rock' and similar.</p>
<b>Private</b>	
Building height	2 storeys generally, 3 storeys at corners and junctions
Plot width	4m minimum
Frontage on building line	80 - 100%
Front garden / setback	2m min - 6.5m for in curtilage parking, with 1m high wall or railings



**Residential Street**  
Footpath and sensitive on-street parking. High quality materials should be used on all surface treatments.

**Suggested Materials Pallet \***

Carriageway Surface	Footway / Cycle Lane Surface	Parking bays / Crossings	Carriageway Kerbs
Bituminous (black / grey)	Textured Concrete Paving (light grey)	Tegula Setts (dark grey)	Textured Concrete Kerb (light grey)
			

\* Developers will be expected to use the materials in the suggested materials pallet, unless suitable alternatives are chosen which either match or exceed the quality of those suggested.



## Home Zones

Home Zones are intended as more intimate roads connecting Residential Streets to the lowest level of road hierarchy, Mews and Courtyards. They should not link directly to the Boulevard or main routes. Road space will be shared between drivers of motor vehicles and other road users, with the wider needs of residents in mind. The aim is to change the way that streets are used and to improve the quality of life in residential streets by making them places for people, not just for traffic. Changes to the layout of the street must emphasise this change of use so that motorists perceive that they should give informal priority to other road users.

### Key Components

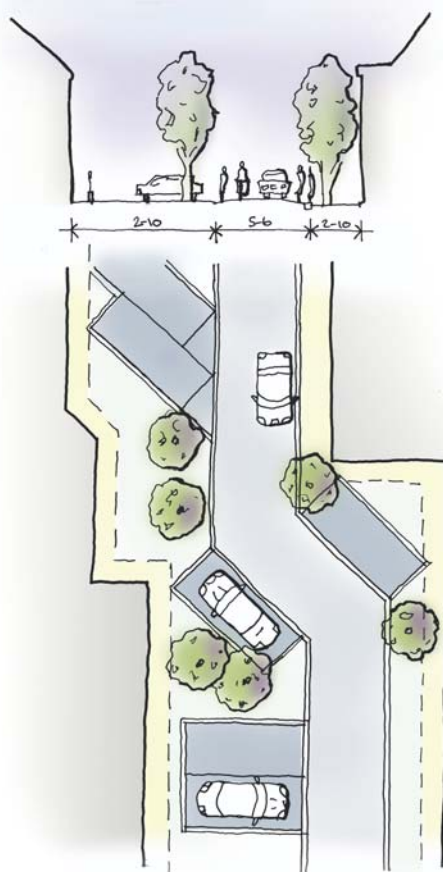
The street treatment is made up of the following key components:

- Tree planting in footways and in build outs wherever widths allow.
- Informal arrangement of street furniture, planting, parking bays and public art.
- Building frontages and entrances to face onto street to create pinch points and activity onto the street.
- Home Zone entrance marked by a change in road surface.
- Use of shared surface treatments incorporating parking areas, pedestrian surfaces and motor vehicle traffic.



## Design Parameters





<b>Public</b>	
Speed limit	10 - 15mph
Carriageway width	5 - 6m pedestrian, cycle and vehicular route, plus bay parking
Corner radii	6m
Tree Planting	<p>Species may be selected from the following; choice depends on location and space. Aim is for some (but not pure) uniformity ie for substantial groups consisting of single species.</p> <p>Large trees:  Acer platanoides 'Columnare'  Carpinus betulus 'Fastigiata'  Tilia cordata 'Green Spire'  Tilia Platyphyllos 'Fastigiata'</p> <p>Medium trees:  Pyrus calleryana 'Chanticleer'  Sorbus x thuringiaca 'Fastigiata'  Acer campestre 'Elegant'  Acer campestre 'Arends'  Sorbus aucuparia 'Joseph Rock' and similar.</p>
<b>Private</b>	
Building height	2 storeys generally, 3 storeys at corners and junctions
Plot width	4m minimum
Frontage on building line	80 - 100%
Front garden / setback	1m min - 6.5m for in curtilage parking, with 1m high wall or railings



### Home Zones

Informal road space shared between drivers and other road users. Motorists should give priority to pedestrians.

### Suggested Materials Pallet \*

Carriageway Surface	Footway Surface (where applicable)	Parking bays / Crossings	Kerbs and Edging
Granite Aggregate Setts (dark grey)	Textured (Conservation) Concrete Paving (grey)	Granite Aggregate Setts (light grey / silver)	Conservation Aggregate Kerb (grey / silver)
			

\* Developers will be expected to use the materials in the suggested materials pallet, unless suitable alternatives are chosen which either match or exceed the quality of those suggested.

## Mews and Courtyards

Mews and Courtyards form intimate spaces located off Local and Residential Streets based on Home Zone principles. Surface treatments, building layout and landscaping should be designed to minimise traffic speed within an environment designed for pedestrians and residents.

### Key Components

The street treatment is made up of the following key components:

- Extensive shrub and tree planting used to define specific spaces and soften the street scene.
- One single point of access. No through traffic or public transport routes.
- Building frontages and entrances to face onto shared surface areas.



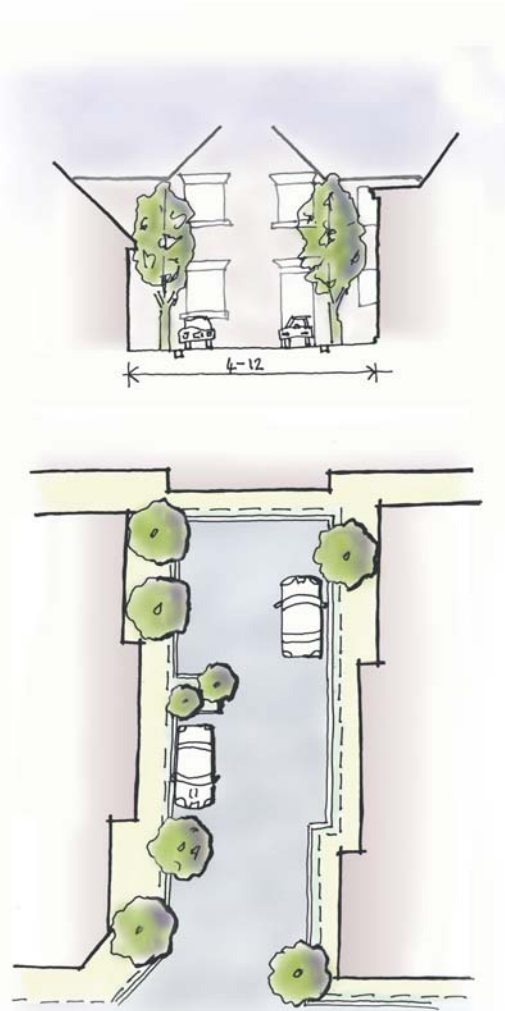
Ivor Samuels



Richard Hanson

## Design Parameters

Public	
Speed limit	15mph
Carriageway width	Irregular formation, combined usage, minimum 3.7m width
Corner radii	6m at junctions
Tree Planting	<p>Species may be selected from the following; choice depends on location and space. Aim is for some (but not pure) uniformity ie for substantial groups consisting of single species.</p> <p>Large trees:  Acer platanoides 'Columnare'  Carpinus betulus 'Fastigiata'  Tilia cordata 'Green Spire'  Tilia Platyphyllos 'Fastigiata'</p> <p>Medium trees:  Pyrus calleryana 'Chanticleer'  Sorbus x thuringiaca 'Fastigiata'  Acer campestre 'Elegant'  Acer campestre 'Arends'  Sorbus aucuparia 'Joseph Rock' and similar.</p>
Private	
Building height	2 storeys generally, 3 storeys at corners and junctions
Plot width	4m minimum
Frontage on building line	80 - 100%
Front garden / setback	2m min - 6.5m for in curtilage parking, with 1m high wall or railings



**Mews & Courtyards**  
Irregular formation of intermit spaces designed for pedestrians and residents.

**Suggested Materials Pallet \***

Carriageway Surface	Footway Surface (where applicable)	Parking bay edging	Kerbs and Edging
Resin bound gravel (sand)	Textured (Conservation) Concrete Paving (grey)	Tegula Cobbles (dark grey / blue)	Conservation Aggregate Kerb (grey / silver)
			

\* Developers will be expected to use the materials in the suggested materials pallet, unless suitable alternatives are chosen which either match or exceed the quality of those suggested.

## Street Dimensions

Some of the projects identified include a range of street types, each with different characteristics, including type of use, width and building heights. A typical distance between frontages in new residential streets should be between 12 and 18 metres and between 7.5 and 12 in a mews. The height of buildings should also be in proportion to the width between frontages or public spaces in order to achieve enclosure. The ratio of width to height will depend on the type of street or open space being designed. Typical widths of streets and height-to-width enclosure ratios are illustrated in the diagrams below.



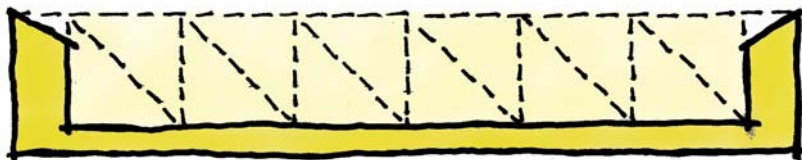
## Width to Height Ratios



Mews 1:1 ratio



Generally effective 1:3 ratio

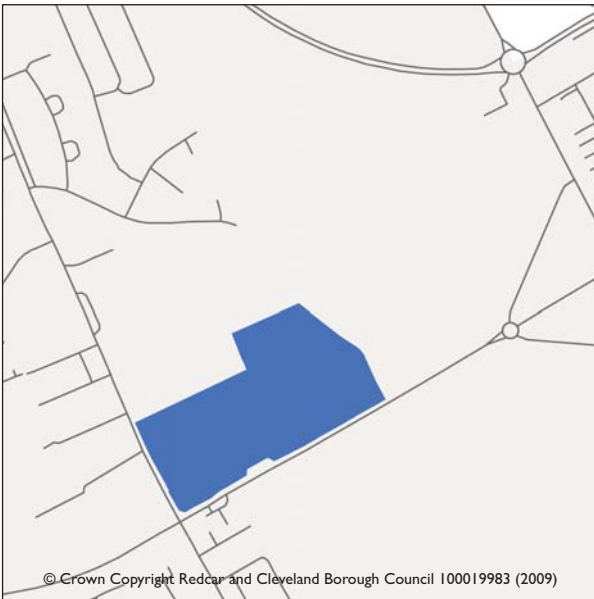


Maximum square 1:6 ratio



Spatial definition by tree canopy

## 4.2 DISTRICT CENTRE



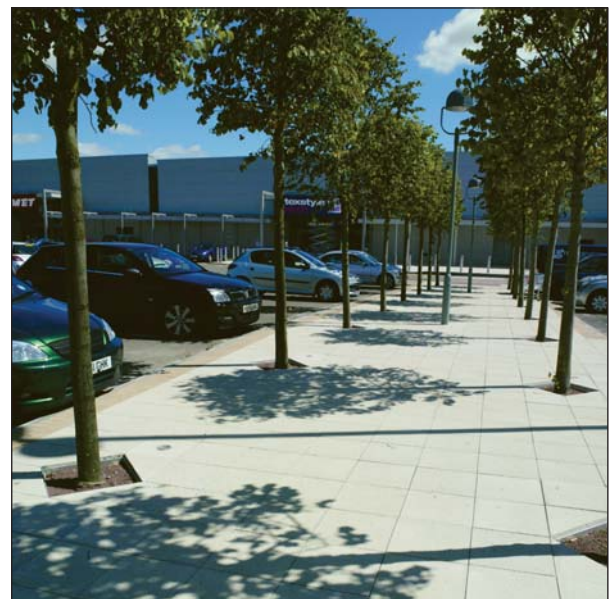
In addition to the types of streets identified in Greater Eston, the District Centre is a key component of the street hierarchy that must respond to its position within the urban form. The design of the District Centre as a place as well as its function in movement terms is critical to the operation and character of Greater Eston.

The District Centre must be designed as a high quality accessible place. Safe pedestrian routes should connect uses within the district centre to nearby car parking as well as major pedestrian routes within the Low Grange development.

### *Key Components*

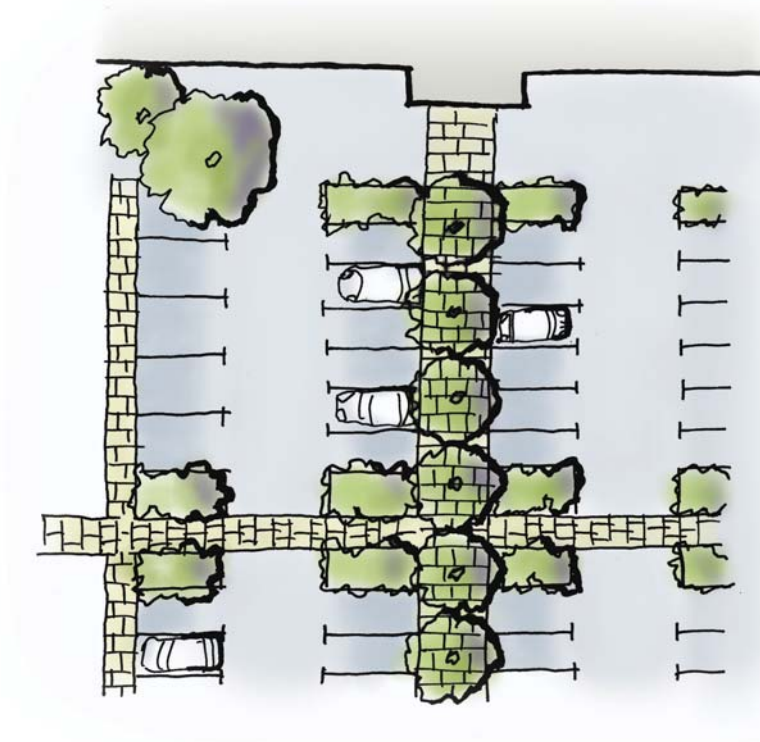
The District Centre is made up of the following key components:

- Extensive tree and shrub planting in car parking areas and along pedestrian and cycle routes.
- Adequate planting along the Trunk Road and Normanby Road to screen views of car parking and to create the Boulevard.
- Consistent high quality hard landscaping materials.
- Clearly defined safe pedestrian routes through car parking areas linking retail to public transport routes.



## Design Parameters

Public	
Speed limit	15 - 20mph
Carriageway width	5m - 7.3m
Footways and Cycle Lanes	2m minimum width + 1.2m cycle lanes where applicable. Cluster of cycle stands at transport hubs and outside retail buildings.
Corner radii	6m typical
Other dimensions	Swept paths sufficient to accommodate retail delivery vehicles and refuse collection on service roads
Parking bays	Formal parking bays provided for retail areas. 2.4 bay width.
Tree Planting	<p>Species may be selected from the following; choice depends on location and space. Aim is for some (but not pure) uniformity ie for substantial groups consisting of single species.</p> <p>Large trees:</p> <ul style="list-style-type: none"> <li>Acer platanoides 'Columnare'</li> <li>Carpinus betulus 'Fastigiata'</li> <li>Tilia cordata 'Green Spire'</li> <li>Tilia Platyphyllos 'Fastigiata'</li> </ul> <p>Medium trees:</p> <ul style="list-style-type: none"> <li>Pyrus calleryana 'Chanticleer'</li> <li>Sorbus x thuringiaca 'Fastigiata'</li> <li>Acer campestre 'Elegant'</li> <li>Acer campestre 'Arends'</li> <li>Sorbus aucuparia 'Joseph Rock' and similar.</li> </ul>
Building height	Building Height 2 - 3 storeys generally, 4 storeys at corners or focal points



**District Centre**  
Car park should screen vehicles and establish direct footways connecting nearby buildings.

### Suggested Materials Pallet \*

Carriageway Surface	Footway and Cycle Lane Surface	Pedestrian Crossings and Markings	Kerbs
Bituminous (black / grey)	Textured Concrete Paving (light grey / silver)	Granite Aggregate Setts (light grey / silver)	Textured Aggregate Kerb (light grey)
			

\* Developers will be expected to use the materials in the suggested materials pallet, unless suitable alternatives are chosen which either match or exceed the quality of those suggested.

## 4.3 PARKING

The level of parking provision and its location has a key influence on the form and quality of a place, and the choices people make in how they travel. The way cars are parked can affect the visual quality, street activity, interaction between people and safety of places.

Careful consideration should be given to the above factors to determine appropriate levels of parking provision in Greater Eston. Where new development is proposed, or where existing housing is to be replaced, parking provision will be expected at a ratio of 2 spaces per dwelling. The parking ratio to be applied to each new development will be dependent on the mix of housing and its location.

A combination of both allocated and non-allocated communal parking should be provided in order to cater for both residents' and visitors' needs and for peaks in demand. Car parking will be provided on or off the street. Off-street parking includes parking within a curtilage (on-plot) or in off-street parking areas (off-plot), such as in Homezones, courtyards and on-street parking.

### Off-Street Parking

Where off-street parking is proposed within the curtilage, it must be kept to a minimum and should not be detrimental to the street scene. Preference should be given to either integral garages or parking at the side or rear of properties which is well set back behind the front building line.

### On-Street Parking

Within residential areas, on-street parking should be well designed to form part of the street layout and pedestrian environment. An arrangement of discrete parking bays adjacent to the carriageway

is often an effective way of providing on street parking either parallel or perpendicular to the street. Parking numbers should be limited to no more than approximately 5 spaces per group. These groups can be separated by kerb build outs, changes in surface treatment, street furniture or planting.

### Courtyard Parking

Where courtyards are proposed within residential areas the following principles will apply:

- Courtyards should be accessible from residential and local streets only at one point of access.
- Courtyards must be secure and accessible only to residents.
- Courtyard design must accommodate vehicular and pedestrian movement. Larger courtyards should incorporate private communal space where possible.
- Courtyards should not exceed more than 10 car parking spaces.
- Non-designated visitor car parking should be provided within the street, not in the courtyard.
- Courtyards should be managed as private space and secured with gates.

### Cycle Parking

Providing enough convenient cycle parking at people's homes and other locations for both residents and visitors is critical to increasing the use of cycles in Greater Eston. Cycle parking should be provided for all new development. Where apartment development is proposed cycle parking should be provided in a shared facility, which is convenient and secure to use. New housing should provide cycle parking within the dwelling, outbuildings or a shared cycle store easily accessible to the dwelling.

## District Centre Parking

Parking in the District Centre should be designed as an integral part of the landscape treatment. Adequate space must be allowed for tree planting and landscaping in all parking areas.

It is important that shared surface treatments are used throughout the district centre to clearly define safe cycle and pedestrian routes through car parking areas. These routes should connect uses within the district centre to nearby car parking as well as major pedestrian routes within the Low Grange Farm development.

Car parking must be adequately screened from the Trunk Road and Normanby Road. Where it is not possible to screen views of car parking with mature trees, other methods should be explored, such as structural screening.

## 4.4 PUBLIC AND PRIVATE SPACE

### Public Space

Green spaces serve a range of functions from providing spaces for play and recreation to areas of woodland habitat for wildlife as well as enhancing environment locality. Each residential community will be linked to the green spaces through a series of well-defined safe streets, incorporating pedestrian friendly routes and cycle ways. These open spaces will be accessible to all, secure and provide quality leisure and amenity facilities. Public space should be well integrated and connected to the existing green infrastructure network.

There will be opportunities for enhancements to the existing open space network and the introduction of new spaces. The following key principles apply to the design of public space:

- Public spaces should be overlooked by housing.
- The function of the space should correspond to the size of the space, minimising conflicts with neighbouring properties.
- All new play equipment should be positioned to accord with the National Playing Fields Association Codes. Play areas of different types should be positioned at the following minimum distances from adjacent dwellings:
  - Local Area for Play (LAP) = 5m
  - Local Equipped Area for Play (LEAP) = 10m
  - Neighbourhood Equipped Area for Play (NEAP) = 30m

(distances are measured from the property frontage).

### Private Space

Private space should principally be in the form of private gardens and shared gardens. In each instance the following principles should be observed:

- The space should be accessible only to the property that owns the space.
- The space should be overlooked by the property that uses the space, with the building design responding to this need.

Where private space abuts the public realm, security and safety considerations are important. The boundary treatment should be well designed and appropriate to the residential character of the area.



# 5.0 DETAILING THE PLACE

## 5.1 BUILDING SIZE

The size and configuration of buildings will have an impact on their sustainability and relationship with the surrounding buildings and public spaces. The scale of buildings should be informed by those in the surrounding area. In general new development should respond to and help maintain a residential scale. Occasional changes in height, width and depth of buildings help to generate architectural interest and variety.

### Building Height

Buildings should not exceed 3 storeys, except at key locations such as the district centre, civic and commercial centres or sites of visual importance where landmark buildings can help to create focal points.

### Corners

Corner sites should be utilised to emphasise nodes and add interest to streetscapes. Buildings on corners need to face two ways by incorporating entrances and windows on both sides of the building.



David Millington Photography Ltd



## 5.2 BUILDING STYLES

### Windows

Designs should be selected that take account of their function and appearance. Windows make an important contribution to the overall appearance of the streetscape and public realm.

Windows and doors play a major role in the identity of buildings and streets, enabling passive supervision and participation between the public and private realm.



Richard Hanson

**Design proposals for windows should comply with the following guidelines:**

*All windows and their subdivisions should relate to the proportioning of the entire building. Windows should generally be vertical.*

*Bay windows should be well designed with respect to proportion, scale and detail.*

*Window design should seek to maximise daylight into buildings to enhance energy saving and wellbeing.*

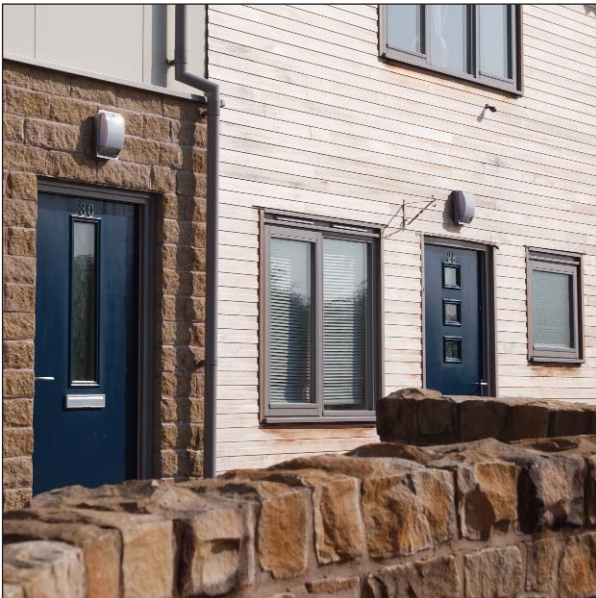
*Bathroom, shower or en-suite windows should not be located on street elevations.*

*Window cills should be provided unless a stringcourse sheds water below the window. On brick or stone buildings, window reveals should be a minimum of 75mm.*

*Window specifications should all comply with the requirements of Secured by Design.*

## Doors

Doors and entrances are equally as important as windows. Bespoke solutions with good proportion for doors and porches that reflect the best of the local vernacular will be encouraged.



Richard Hanson



Ivor Samuels

### **Design proposals for doors should comply with the following guidelines:**

*Door reveals should be a minimum of 75mm, which may be achieved by using small hardwood cills with subcills.*

*Garage doors for mews houses and within courtyards should be non-projecting, so as not to present an obstacle to pedestrians or cars.*

*Where porches or canopies are proposed, they should be designed as part of the building and relate closely to the style of the building. Roofing on porches should match the roofing material of the principal structure.*

*The design and materials of doors should complement the design of the building. Doors on traditionally designed buildings and those designed to follow the existing built form should be painted timber. Modern materials will only be appropriate on buildings which have a contemporary design.*

*Columns, posts or piers must be robust. For example, masonry piers shall be no less than 450mm and timber piers shall be no less than 150mm x 150mm.*

## Roofs

Roofs can help to add visual interest and variety to the skyline. They can help to define vistas and focal points or to convey particular activities and concentrations of uses, such as a central square or cluster of commercial buildings indicating a retail centre. Rooflines also offer an opportunity to emphasise natural forms, particularly on sloping sites, to make a place of distinction.

### ***Design proposals for roofs should comply with the following guidelines:***

*Roofs should be appropriately scaled and proportioned.*

*Roofing materials should relate to the design of the buildings of which they would form a part. More modern and contemporary designs, including the use of 'green roofs', will be promoted.*

*Gutters and down pipes must be integrated by careful design.*

*The treatment of eaves should relate directly to the proposed building type and style.*

*If dormers are used, they should always suit the roof they sit on in terms of scale and materials. If the dormer roof is gabled rather than hipped, the verge should be made disproportionately deep (75mm or 100mm) to give a deeper shadow, with the tile battens running past the line of the gable end.*



## Walls

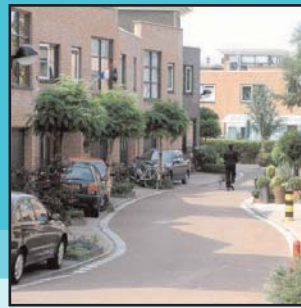
There is a wide range of materials used for walls in buildings in Greater Eston. Materials used in the construction of walls will strongly influence the perception and experience of place and assist in defining durability, longevity and legibility of buildings and boundaries. The Code does not prescribe materials to be used, but gives guidance on achieving high quality.

### ***Proposals for the construction of walls should comply with the following guidelines:***

*The choice of materials used in the construction of walls will be driven by the design ethos of the buildings themselves. More modern and contemporary designs are encouraged, providing that they sit comfortably within their surroundings.*

*Where traditionally designed buildings are provided, appropriate materials and building methods should be used, which are in keeping with the local character.*

*Design teams should consider materials which respect place and good building practice, both from construction and sustainability considerations.*



## 5.3 PUBLIC REALM

The public realm should be a comfortable and stimulating place that encourages social interaction. This requires detailed attention to the structure of the space and the elements it contains. Consideration should be given to the following elements:

- Hard and Soft Landscaping
- Pedestrian and Vehicle Surfaces
- Security
- Public Art
- Street Furniture
- Lighting
- Signage

### Public Art

Public art will make a major contribution to Greater Eston by providing a place of character and quality when integrated into the overall design approach. A Public Arts Strategy for Greater Eston will be prepared to provide specific guidance and good practice on public art. The Public Arts Strategy should be used alongside the Design Code to ensure that art is fully integrated into the design of developments in Greater Eston.

The Council is committed to public art, which contributes to:

- The development of culture and social interaction;
- The promotion of community involvement, learning and interaction with the environment;
- The enhancement of quality of life by contributing to safety, security and community cohesion;
- The creation of a place of distinct character and quality.



## Street Furniture

Public space is occupied by a host of components, which include seats, fences, shelters, poles, lights, bollards, CCTV, signs and cycle stands. The public realm should be designed and managed in such a way as to ensure that all street furniture is of a co-ordinated design and avoids creating a cluttered appearance. This should involve a partnership with key stakeholders, including the highways authority, the local authority and utilities companies where appropriate.



### ***Proposals for the use and placement of street furniture should comply with the following guidelines:***

*Give special consideration to the location of street furniture and direction of pedestrian movement in order to maintain clear, unimpeded access for the elderly, and the visually and mobility impaired.*

*Ensure signage is kept to a minimum by removing any superfluous components.*

*Ensure signage is robust, high quality and durable, with hidden/recessed anti-vandal fixings.*

*Fix signage and litter bins to existing poles/posts such as lighting columns, wherever possible.*

*Overall, signage should be kept to a minimum and used only as required.*

*Ensure public transport elements such as bus shelters and seating are consistent in style and colour.*

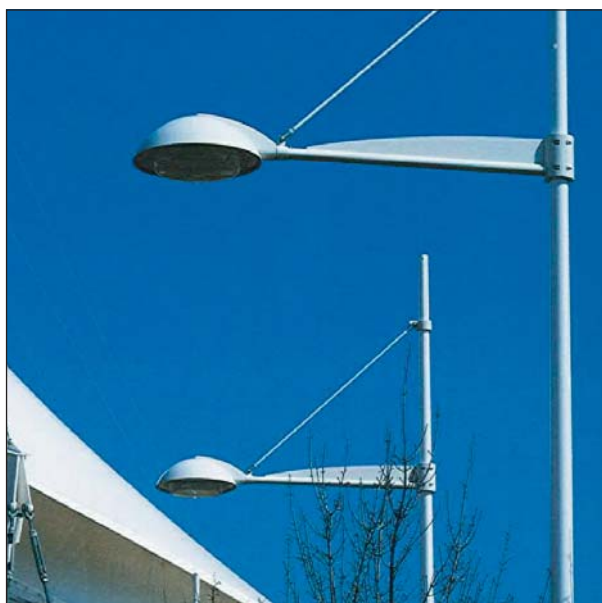
*Avoid the over-use of bespoke items and use where possible established manufacturers and suppliers of street furniture.*

*Ensure that the choice of material takes reference from the selections proposed in the Chapter 4 – Spatial Structure.*

## Lighting

Lighting throughout Greater Eston should be designed to be imaginative and effective whilst creating a safe environment that encourages people to use streets, squares and open spaces after dark.

Good exterior and area lighting will play a key role in the way people use and perceive their environment by helping to create a sense of local identity and civic pride. Feature lighting will be encouraged to help to define and enhance areas of public importance and interest.



### **Proposals for the placement and selection of lighting should comply with the following guidelines:**

*Ensure lighting on private buildings is integral to the overall design of the building elevation.*

*Where appropriate, attach lighting units to buildings to reduce street clutter.*

*Ensure that the style and colour of lighting is appropriate to its context and that lighting design has continuity throughout the neighbourhood.*

*All street lighting should be in accordance with the Council's approved specification. Please contact the Council's street lighting section for details.*

*Ensure that the scale and position of lighting is relative to the surrounding buildings and spaces.*

*Illuminate both the carriageway and footpath, including traffic calming features and obstacles.*

*Ensure that shadows and dark areas are avoided in streets where pedestrians may be vulnerable.*

*Use changes in light source to distinguish variations in hierarchies of street type. For example white lighting used to distinguish residential areas from main traffic routes which use sodium lighting (orange coloured).*

*Coordinate lighting with other materials and street furniture.*

## Signage

Signage should be designed to convey directional and orientation information to visitors and residents, but also provide useful and meaningful information that is of interest. It is important that designers refer to the Department for Transport, Traffic Signs Manual (2006), before embarking on the design of signage. In residential areas minimal signage should be used in order to reduce clutter. Where existing neighbourhoods are being redesigned streets should be subject to a signs audit to ensure that they are not over signed. Redundant signs should be removed.



### **Proposals for signage should comply with the following guidelines:**

*Ensure the size of the sign suits the speed of the traffic regardless of its purpose. If the sign is necessary, motorists need to be able to read it.*

*Remove existing signs which have no clear purpose to reduce clutter and to ensure that essential messages are prominent. Each and every sign should be necessary.*

*Where appropriate, incorporate colour contrast bands on poles and columns to help partially sighted people.*

*Use features such as public art, architectural styling, planting and focal points to help assist navigation.*

*Fix signs to walls or existing poles wherever possible. If more than one sign is required they should be grouped together on a single post.*

*Attach street nameplates to buildings rather than poles, particularly in new residential areas.*

*Ensure signage is consistent and coordinated across Greater Eston. Signs must be legible with clear typeface and tonal contrast.*

*Encourage non-statutory signs that contribute to the sense of place. These may include gateway signs and features.*

*Ensure signage on private buildings is integral to the overall design of the building elevation.*



# 6.0 PERFORMANCE CRITERIA

There are a number of criteria against which the design of developments in Greater Eston will be measured. This section is broadly in line with English Partnerships' Quality Standards as set out in their policy guidance document 'Delivering Quality Places'.

## DESIGN AND ACCESS STATEMENT

A design and access statement should be developed at the inception of any design and build project. This will identify the opportunities for good design in a specific context and help deliver quality developments in Greater Eston more quickly and efficiently. The design and access statement should be used to demonstrate an understanding of site briefs, the Design Code itself and the developer's interpretation and response to those objectives.

The design and access statement should adhere to the design principles and processes outlined in the Urban Design Compendium 2: Delivering Quality Places published by English Partnerships and the Housing Corporation. As a minimum the design statement should include the following sections:

- Appreciating the context
- Creating the urban structure
- Making the connections
- Detailing the place
- Delivering quality places
- Integrated design
- Implementation and delivery
- Managing quality places

The design and access statement should take into account the elements set out in the Design Code for Greater Eston and should explain design principles in terms of layout, density, scale, landscape and visual appearance. Full



specifications of the public realm must be included in order that a true comparative assessment can be carried out for each development.

Access needs should be considered from the outset of a development and integrated into design. The design and access statement will be required to outline the measures taken to achieve an environment that is accessible and appealing to all, regardless of physical ability, age, gender or circumstance.

## BUILDING FOR LIFE

The Building for Life standard represents the national standard for the design of housing and neighbourhoods. It is awarded to new and refurbished housing projects and mixed-use communities that demonstrate a commitment to high design standards and place making.

The Silver Standard within Building for Life is the minimum acceptable quality of development for housing and neighbourhoods. Projects are judged against 20 criteria and projects that achieve 14 out of the 20 are deemed to have reached the

Silver Standard. The 4 main areas judged under this standard are:

- Character
- Roads, parking and pedestrianisation
- Design and construction
- Environment and community.

The Design and Access Statement must outline how the 20 criteria under these 4 main areas have been addressed and how the developer intends to achieve the award upon completion.

## SECURED BY DESIGN

Developments must be designed in line with Secured by Design. Secured by Design accreditation is a Police initiative that encourages the building industry to adopt minimum standards in designing safe and secure developments. Equal weighting is given to the importance of environmental design and physical security.

At the design and planning stage of development the Police Architectural Liaison Officer will be asked to provide written evidence of the scheme's likelihood of achieving accreditation upon completion. Developers should outline in their design and access statement how they have responded to the principles of Secured by Design as set out below:

- Natural surveillance
- Defensible space
- Community interaction
- Crime prevention through environmental design.

## INTEGRATION OF TENURE

The housing developments in Greater Eston must provide a mix of different tenures and forms of ownership that reflect different needs and requirements of the community and individuals.

Development and design should be 'tenure blind' in that the type of tenure cannot be deduced from the design, quality, location within the site, timing of development or by significant difference in the access to services and amenities.



*David Millington Photography Ltd*

## CODE FOR SUSTAINABLE HOMES

The Code for Sustainable Homes is a national sustainability standard for new homes. The Code has six levels with level one being the entry level, above the level of Building Regulations, to level six, which is a zero carbon development achieving exemplar development in sustainability terms.



The Code measures the sustainability of a home against nine design categories, rating the 'whole home' as a complete package. The design categories are:

- Energy and CO<sub>2</sub> emissions
- Water
- Materials
- Surface water run off
- Waste
- Pollution
- Health and wellbeing
- Management
- Ecology

It will be a requirement that all new homes achieve a minimum of level 3 initially. This will be raised to Code Level 4 from April 2010 and Code Level 6 from April 2013. These Codes will apply for all construction starting after these dates.

Further information can be found in the Communities and Local Government publication, Code for Sustainable Homes: Setting the Standard in Sustainability for New Homes (2008).

## LIFETIME HOMES

For homes to be sustainable in the long-term they need to be capable of adaptation to meet the changing needs of users during different periods in their lifetimes. This increases the longevity of tenure, vital to individual and community well being.

The 16 standards set out in Lifetime Homes 21st Century Living: Quality, Flexibility and Choice cover issues such as:

- Car parking
- Access
- Approach and entrance
- Internal movement
- Flexibility
- Internal specification

The Code for Sustainable Homes will include the principles and standards of Lifetime Homes as part of the ratings process.

Lifetime Homes is an option to gain points for relevant rating, however is not compulsory.

## NOISE

Inadequate sound insulation between homes can be a source of complaint by residents, and can have a considerable impact on an individual's satisfaction with their home. Developers working

in Greater Eston must achieve higher levels of sound insulation than the minimum required in Approved Document Part E of the Building Regulations:

- Airborne sound insulation values must be at least 5dB higher than that required in the current Approved Document Part E.
- Impact sound insulation values must be at least 5dB lower than the performance standards set out in the current Approved Document Part E.

## SPACE STANDARDS

It is recommended that the following minimum space standards are applied:

### Apartments

1 bedroom/2 person apartments	40 sqm
2 bedroom/3 person apartments	52 sqm
2 bedroom/4 person apartments	55 sqm

### Houses

2 bedroom/4 person homes	57 sqm
3 bedroom/5 person homes	71 sqm
4 bedroom/6 person homes	90 sqm

## WASTE AND RECYCLING

The storage and collection of rubbish and materials for recycling must also be carefully considered in the design of blocks and buildings.

The general guidelines for the storage and collection of rubbish and materials recycling are as follows:

- The design of storage facilities for waste and recycling materials needs to be flexible to accommodate changing priorities, technologies and the extent of the recycling programme;
- For apartments, provision should be made to store refuse bins for recycling materials and waste out of public view;
- For all homes with gardens, well designed refuse storage should be provided in covered areas capable of accommodating refuse bins

for general waste and sorted materials for recycling collection;

- Opportunities for communal recycling facilities should be explored;
- Moving the waste and recycling bins to the public footpath will be the responsibility of the occupiers and this should be considered;
- Easy access should be provided from the bin storage area to the street;
- In all situations, refuse and materials recycling storage facilities (whether residential or commercial) must be protected from the weather and be designed as an integral part of the built form of development proposals. There should be no adverse impact on the public realm from the recycling and refuse facilities; and
- The choice of materials and overall design of waste storage facilities should complement the built development proposal.



## SUSTAINABLE DRAINAGE SYSTEMS (SuDs)

Wherever practicable, developments in Greater Eston should seek to provide drainage installations that minimise the flow of surface water into mains drainage systems. This is intended to maintain the natural water cycle while reducing the demands on mains infrastructure.

Greater Eston is a largely urban area, but does have large areas of open space, some of which will be developed, such as Low Grange Farm. It is therefore important that SuDs are used in new developments to ensure that new development does not adversely impact upon the drainage infrastructure. Every development will be expected to provide at least one of the following:

- A water butt for every dwelling with a garden;
- ‘Green’ roofs, especially on larger low pitched roofs, or roofs which will be overlooked;
- Permeable paving or roadway installations on unadopted areas, especially areas of block paving, subject to ground conditions and building proximity.

Further guidance on SuDs is available PPS25 ‘Development and Flood Risk’ and in the Interim Code of Practice for SuDs produced by Ciria.



# APPENDIX I - DESIGN CODE CHECKLIST

<b>Layout and Arrangement</b>
Are dwellings orientated to maximise daylight and passive solar gain?
Does the layout promote a high degree of pedestrian and cycle access within the proposed development and adjoining areas?
Is the proposed layout easy to understand by visitors and easy to find your way around?
Are streets and open spaces well overlooked by buildings?
<b>Private Open Space</b>
Does public open space encourage frequent use? Is it easily accessible, adequately lit, sheltered and functional?
Does the layout promote privacy and ownership of private spaces?
Are open spaces well overlooked and well dispersed throughout the development?
Do open spaces utilise natural daylight?
Is private open space both usable and accessible?
<b>Housing Design</b>
Do building materials respect and complement the surrounding area while remaining compatible within a contemporary context?
Do house types provide natural ventilation and day-lighting?
Have energy and water saving technologies been incorporated into the design of housing?
Have materials been locally sourced?

Are bin stores and recycling facilities included within the proposed development?

### **District Centre Design**

Does the District Centre connect with adjoining neighbourhoods and the Urban Park; are the physical connections legible and convenient?

Is the District Centre pedestrian friendly and not dominated by cars?

Do the proposed buildings enclose and define the retail centre and create a usable public space?

Has the design of the proposed buildings had regard to future adaptability and alternative uses?

Is car parking adequately screened to minimise the impact of vehicles on the streetscape/Boulevard?

Is the proposed public space an attractive social space, focal point and events space?

Are materials of a high visual quality and durability?

Are large parking areas set well back from the building line and away from the public realm?

Will the uses proposed within the District Centre provide a vibrant urban public realm?

### **Landscape and Public Open Space**

Do circulation patterns enhance the public realm by connecting spaces along safe and well overlooked routes?

Is the proposed development sensitive to existing wildlife habitats and species, and does it incorporate opportunities to protect and enhance biodiversity?

Do the proposals conserve and enhance local landscape character?

Do proposals promote the provision of open space and recreation amenities?

Are proposals well linked to green corridors and/or open spaces in adjoining neighbourhoods?

### Landscape and Public Open Space (Continued)

Is the use of outdoor furniture, lighting and seating proposed?

Does the design and positioning of open spaces promote and utilise natural passive surveillance?

Do the proposals provide for native woodland planting with potential to provide shelter, bio-diversity and amenity value?

Does design promote low maintenance care?

Are Sustainable Urban Drainage Systems proposed?

### Green Networks

Do the width and positioning of pedestrian and cycle routes respond to their location and function?

Do proposals enhance the existing network of pedestrian and cycle routes throughout Greater Eston?

Is adequate width provided along paths for pedestrians and cyclists to pass one another?

### Parking and Accessibility (Residential)

Are in-curtilage parking spaces and garages set well back from the building line?

Is off street car parking promoted as an integral part of the urban streetscape?

Are shared communal car parking area encouraged?

Have the principles of Manual for Streets been taken into account in the design and layout of the proposals?

Are shared parking areas secure, well-lit and overlooked by building frontages?



# APPENDIX 2 - BUILDING FOR LIFE QUESTIONS

## Character

Does the scheme feel like a place with a distinctive character?

Do buildings exhibit architectural quality?

Are streets defined by a well-structured building layout?

Do the buildings and layout make it easy to find your way around?

Does the scheme exploit existing buildings, landscape or topography?

## Roads, parking and pedestrianisation

Does the building layout take priority over the roads and car parking, so that the highways do not dominate?

Are the streets pedestrian, cycle and vehicle friendly?

Is the car parking well integrated and situated so it supports the street scene?

Does the scheme integrate with existing roads, paths and surrounding development?

Are public spaces and pedestrian routes overlooked and do they feel safe?

## Design and Construction

Is the design specific to the scheme?

Is public space well designed and does it have suitable management arrangements in place?

Do buildings or spaces outperform statutory minima, such as Building Regulations?

Has the scheme made use of advances in construction or technology that enhance its performance, quality and attractiveness?

Do internal spaces and layout allow for adaptation, conversion or extension?

### **Environment and community**

Does the development have easy access to public transport?

Does the development have any features that reduce its environmental impact?

Is there a tenure mix that reflects the needs of the local community?

Is there an accommodation mix that reflects the needs and aspirations of the local community?

Does the development provide (or is it close to) community facilities, such as a school, parks, play areas, shops, pubs or cafés?



This information is available on request in other languages, in Braille, on tape and in Large Print. For further information contact 01642 774774.

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Bi daxwazê va ev agahî bi zimanên din, bi Braille\*, li ser kasetan û bi Tip û Herfên Mezin heye. Ji bo bêtir agahî, peywendî bi telefona 01642 774774 dahînin. Braille\*(şiklê ko kesê nikarin baş bibînin dikarin pê bixwînin)

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